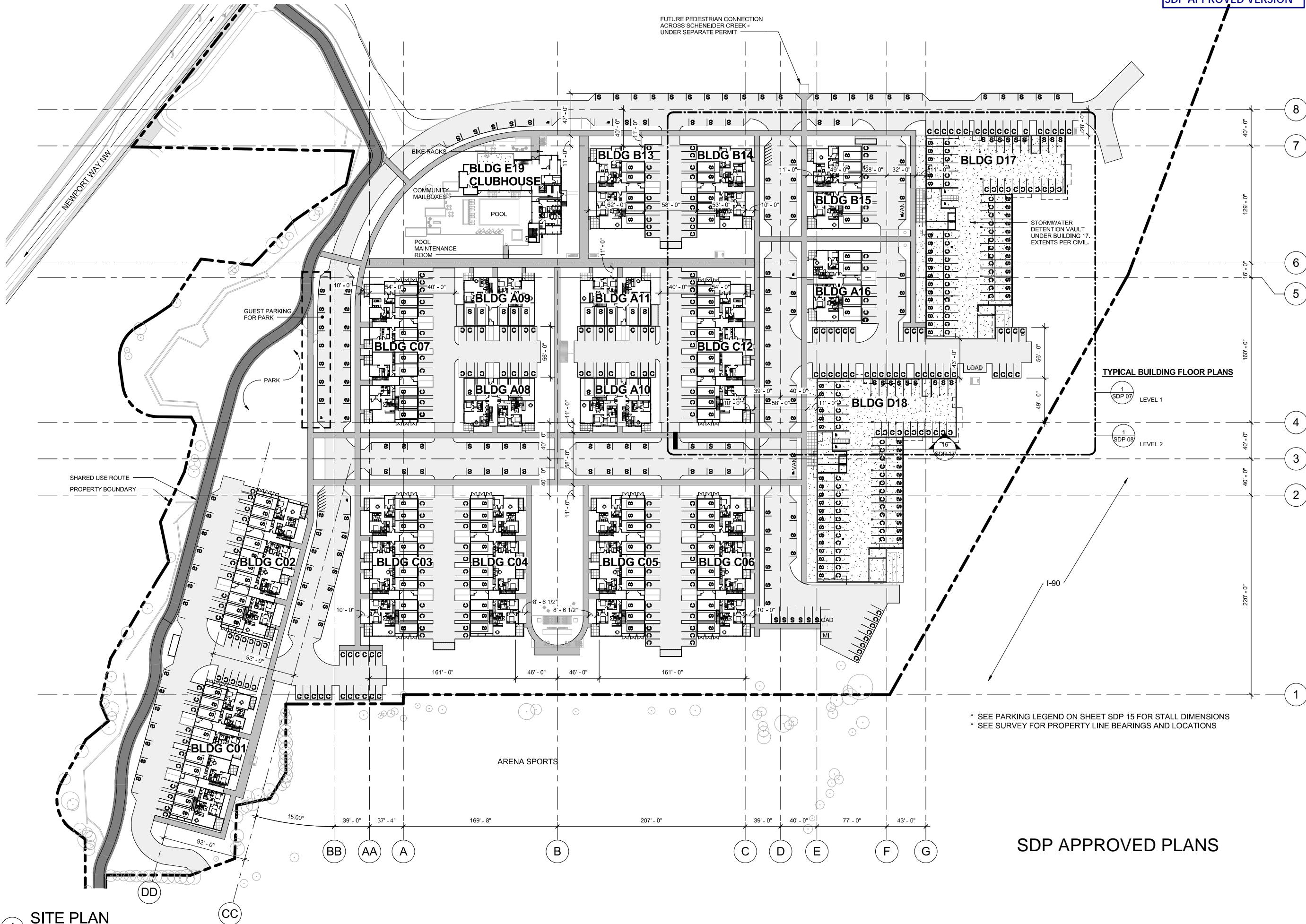


REVISIONS		
No.	DATE	DESCRIPTION
1	4/8/2016	D17 PERMIT CORRECTIONS 1



SDP APPROVED VERSION

TYPICAL BUILDING FLOOR PLANS

1
SDP 07
LEVEL 1

1
SDP 08
LEVEL 2

* SEE PARKING LEGEND ON SHEET SDP 15 FOR STALL DIMENSIONS
* SEE SURVEY FOR PROPERTY LINE BEARINGS AND LOCATIONS

SDP APPROVED PLANS

1 SITE PLAN
1" = 50'-0"

CONSULTANT

PROJECT
ISSAQUAH GATEWAY

2290 NEWPORT WAY NW ISSAQUAH, WA 98027

76314

OWNER



PROFESSIONAL SEAL

DESIGN TEAM:

AH

PRINCIPAL

BM

PROJECT MANAGER

BM

PROJECT ARCHITECT

DRAWN BY

CHECKED BY

DRAWING SET DESCRIPTION

SITE DEVELOPMENT
PERMIT-REV1

REVISIONS

No.	DATE	DESCRIPTION

SHEET TITLE

SITE PLAN



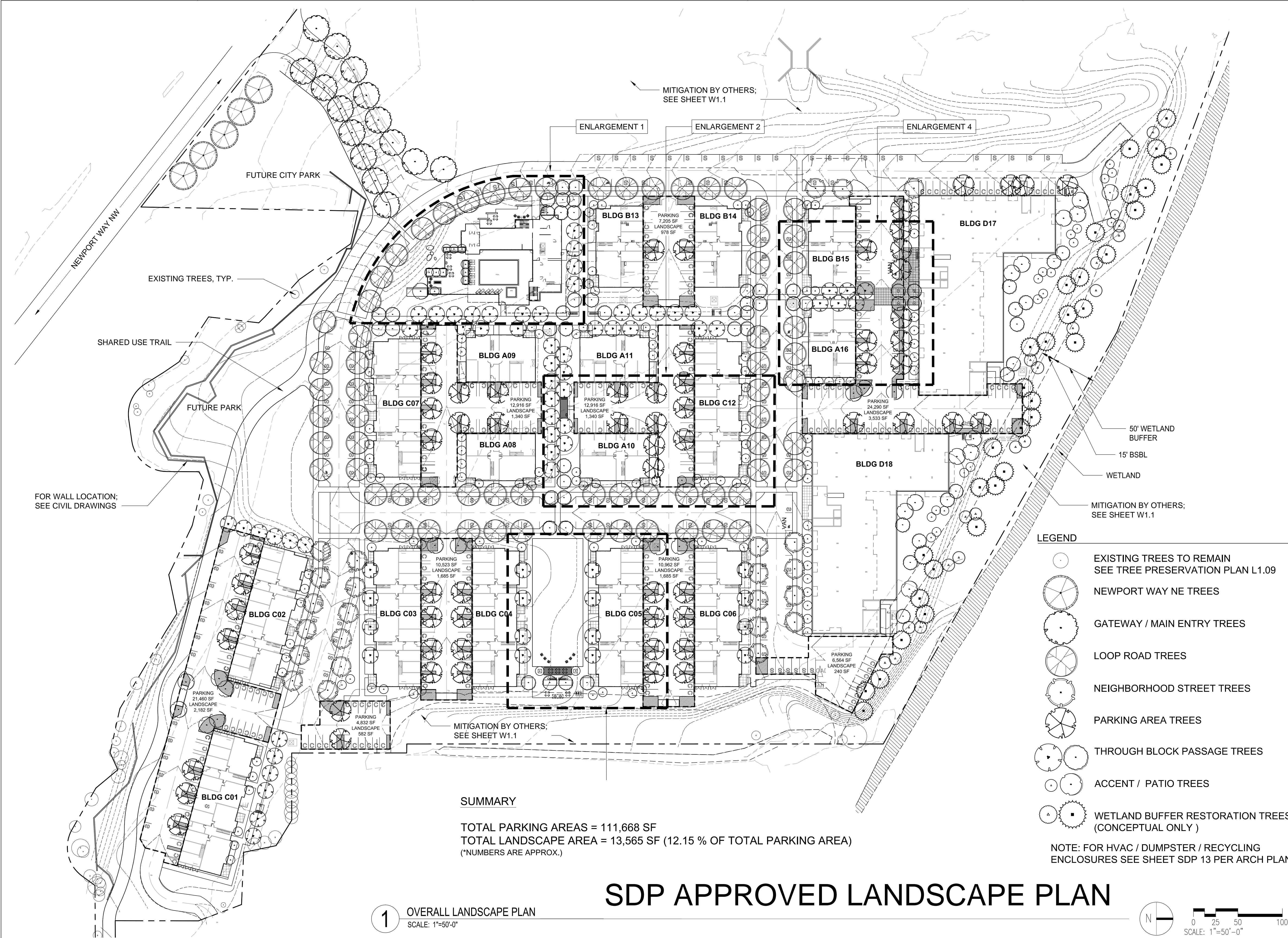
SHEET NUMBER

SDP 01

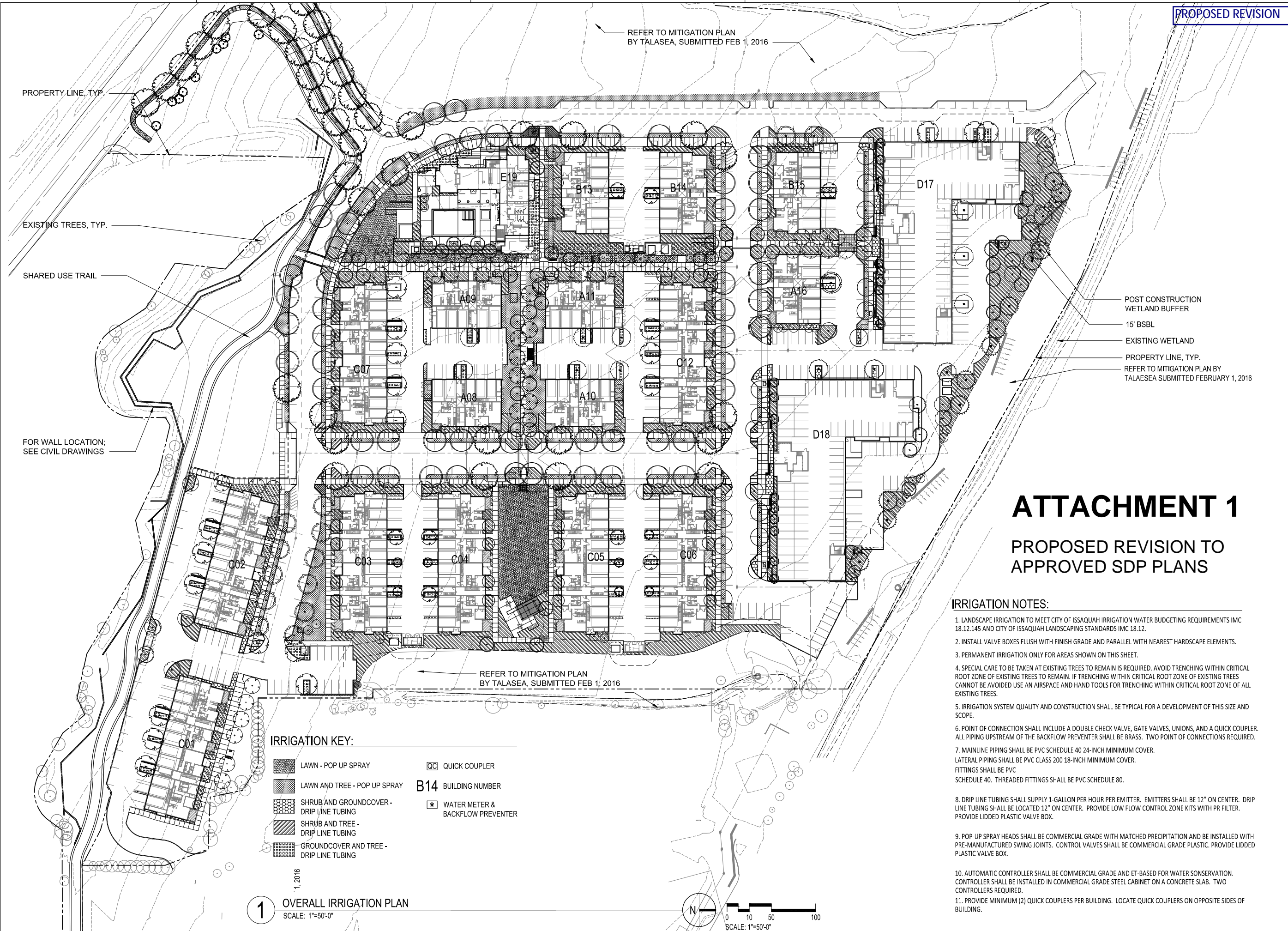
ISSUE DATE

07/07/2015

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OF VIA ARCHITECTURE INCORPORATED.



REVISIONS		
No.	DATE	DESCRIPTION
01	07/06/2015	
02	07/20/2015	



REVISIONS		
No.	DATE	DESCRIPTION

REVISIONS		
No.	DATE	DESCRIPTION

NOTES:

1. FOR UNDERSTORY PLANTING DETAILS AS NOTED IN PLANTING LEGEND.
2. ALL PLANT MATERIALS WILL BE STAKED IN FIELD FOR ACCEPTANCE BY LANDSCAPE ARCHITECT.
3. ALL PLANT BEDS WILL RECEIVE SOIL PREPARATION
4. REFER TO L1.59 FOR PLANT SCHEDULE.
5. ALL TREES TO MEET COI CALIPER AND HEALTH REQUIREMENTS.
6. UTILITIES ARE SHOWN FOR COORDINATION ONLY, REFER TO CIVIL DRAWINGS.
7. REFER TO PLANT SCHEDULE FOR SPECIFIC PLANT MIXES.

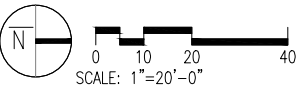
POST CONSTRUCTION
WETLAND BUFFER
15' BSBL
EXISTING WETLAND
PROPERTY LINE, TYP.
REFER TO MITIGATION
PLAN BY TALAESIA
SUBMITTED 02/01/2016

PLANT LEGEND

- TYP. PEDESTRIAN CORNER - SEE DETAIL 5/L5.00, 6/L5.00
- TYP. PEDESTRIAN INTERSECTION / END - SEE DETAIL 7/L5.00
- RESTORATION SEEDING
- MULCH
- LAWN

TREE LEGEND

- SIGNIFICANT TREES TO REMAIN AND PROTECT IN PLACE INSIDE PROPERTY LINE
- SIGNIFICANT TREES TO REMAIN AND PROTECT IN PLACE OUTSIDE PROPERTY LINE
- PROPOSED TREES
FOR TREE LOCATION AND QUANTITY SEE PLANTING SCHEDULE - SHEET L1.59



MATCH LINE SEE L1.53

MATCH LINE SEE L1.55



VIA Architecture | www.via-architecture.com
1809 7th Avenue Ste. 800 Seattle WA 98101
tel 206 284 5624 fax 206 624 5624

CONSULTANT

communita • atelier ps

1402 3rd Ave Suite 1000
Seattle WA 98101

PROJECT

ISSAQUAH GATEWAY

2300 NEWPORT WAY ISSAQUAH, WA 98027

76314

OWNER

THE WOLFF
COMPANY

PROFESSIONAL SEAL



CONCEPTUAL - NOT
FOR CONSTRUCTION

DESIGN TEAM:

LF, NH, LN, KC

PRINCIPAL

LF

PROJECT MANAGER

NH

PROJECT ARCHITECT

JB, BM, BL

DRAWN BY

NH, KC

CHECKED BY

AS

DRAWING SET DESCRIPTION

DESIGN
DEVELOPMENT

REVISIONS

No.	DATE	DESCRIPTION

SHEET TITLE

PLANTING PLAN

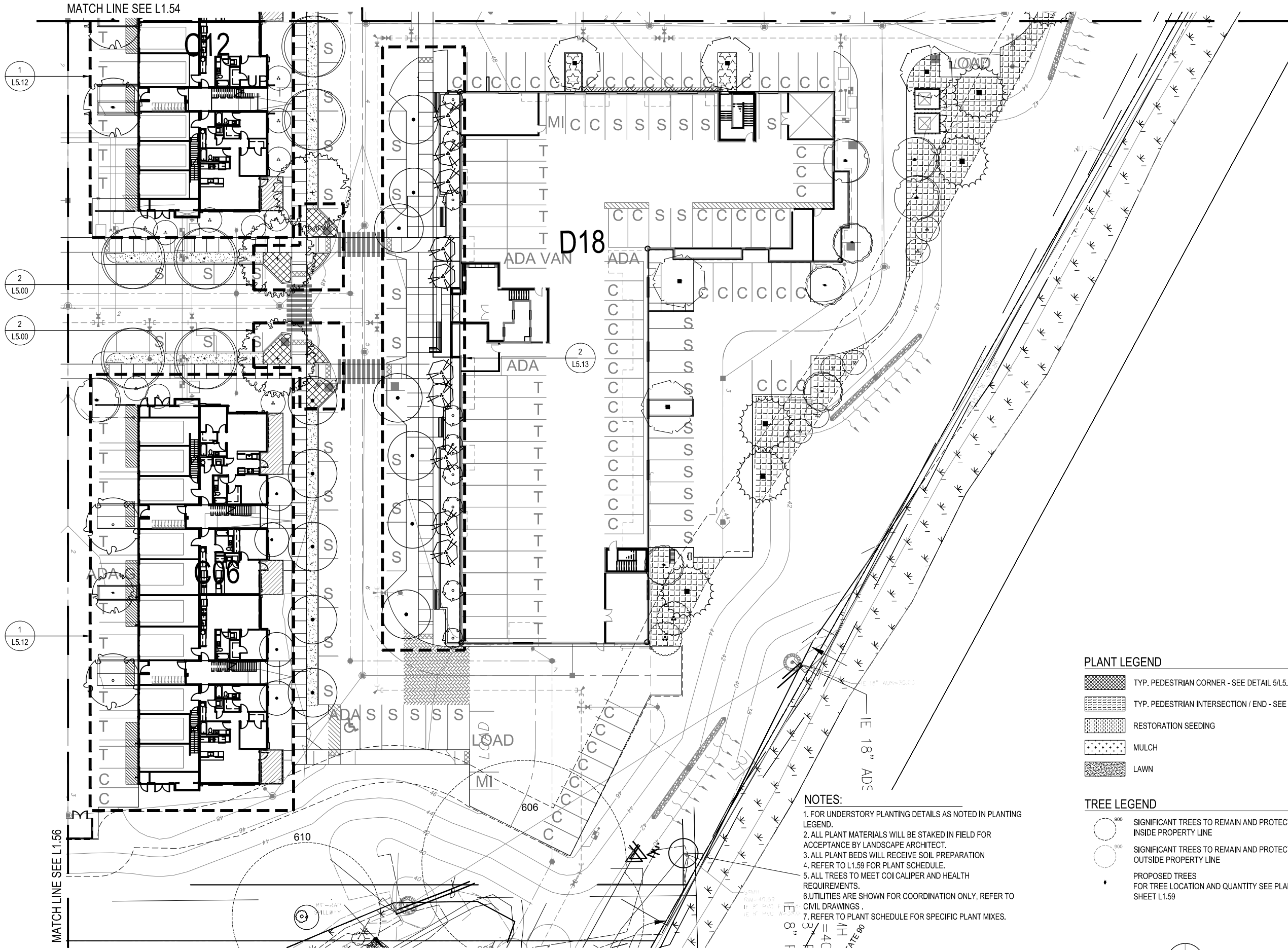
SHEET NUMBER

L1.55

ISSUE DATE

05/24/2016

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PLANT LEGEND

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- TYP. PEDESTRIAN INTERSECTION / END - SEE DETAIL 7/L5.00
- RESTORATION SEEDING
- MULCH
- LAWN

TREE LEGEND

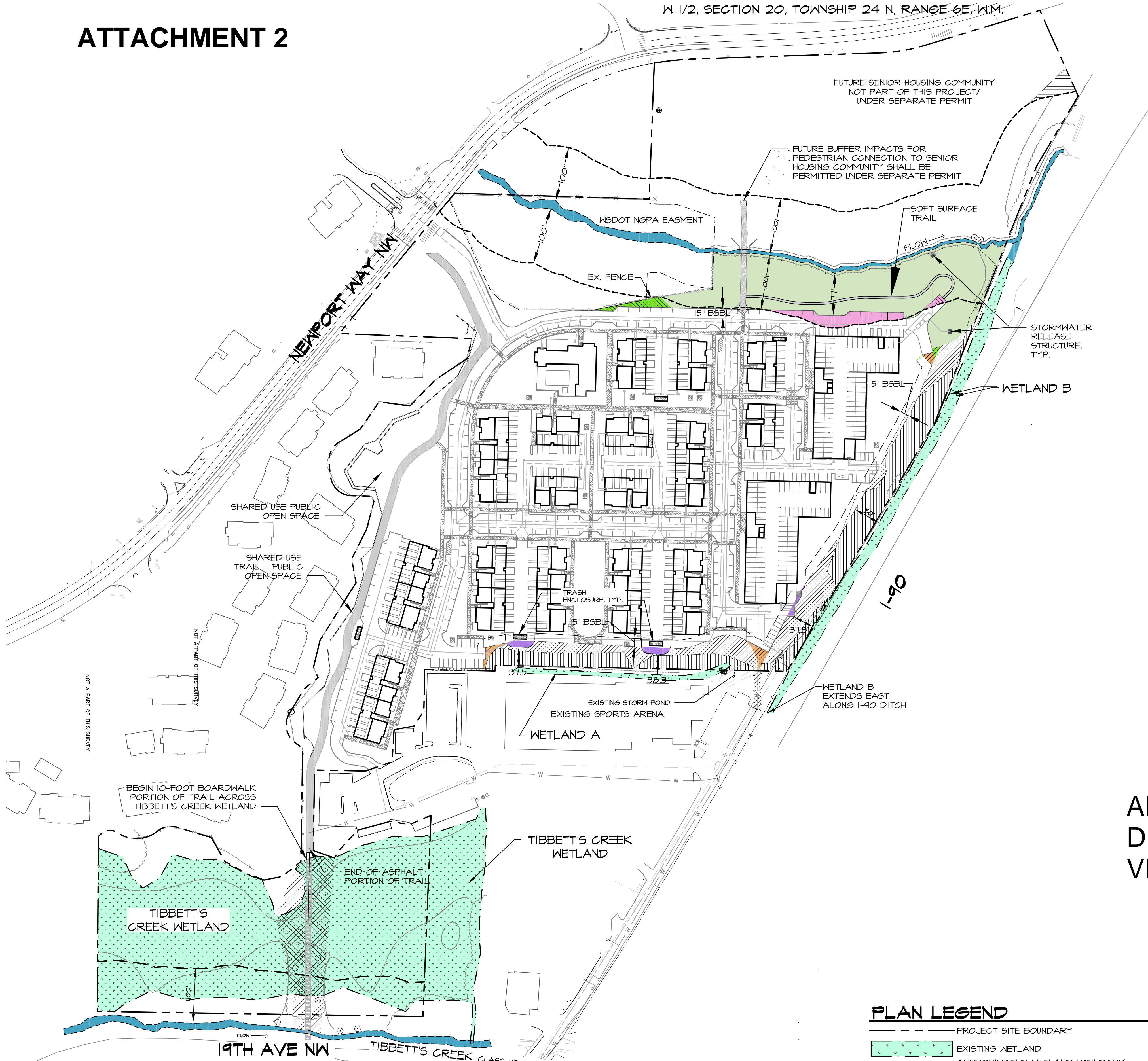
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- UTILITIES ARE SHOWN FOR COORDINATION ONLY, REFER TO CIVIL DRAWINGS.
- REFER TO PLANT SCHEDULE FOR SPECIFIC PLANT MIXES.

1 PLANTING PLAN
SCALE: 1"=20'-0"

ATTACHMENT 2







BUFFER AVERAGING LEGEND

WETLAND BUFFER

REDUCED WETLAND BUFFER AREA	
WETLAND A BUFFER	1,056 SF
WETLAND B BUFFER	354 SF
TOTAL:	1,410 SF




SCHNEIDER CREEK BUFFER

	REDUCED SCHNEIDER CREEK 100-FOOT BUFFER	4,650 SF
	REPLACED SCHNEIDER CREEK 100-FOOT BUFFER AREA	4,807 SF
	REDUCED BUFFER AREA FOR SOFT-SURFACE TRAIL	1,772 SF
	REPLACED BUFFER AREA FOR SOFT-SURFACE TRAIL	1,772 SF

BUFFER IMPACTS LEGEND

TEMPORARY CONSTRUCTION RELATED IMPACTS (GRADING/UTILITIES)	29,028 SF
---	-----------

BUFFER MITIGATION LEGEND

	WETLAND AND STREAM BUFFER RESTORATION FROM TEMPORARY CONSTRUCTION IMPACTS	29,028 SF
	SCHNEIDER CREEK BUFFER ENHANCEMENT	50,905 SF
	WETLAND BUFFER ENHANCEMENT	33,931 SF

SHARED-USE PUBLIC TRAIL MITIGATION

BOARDWALK - INDIRECT CRITICAL AREA IMPACTS		
	INDIRECT WETLAND IMPACTS	4,000 SF
	INDIRECT BUFFER IMPACTS	1,000 SF
	WETLAND ENHANCEMENT (FOR INDIRECT IMPACTS FROM BOARDWALK) 4:1 (4,000 SF x 4 = 16,000 SF)	16,000 SF
	BUFFER ENHANCEMENT (FOR INDIRECT IMPACTS FROM BOARDWALK) 4:1 (1,000 SF x 4 = 4,000 SF)	4,000 SF

APPROVED SITE DEVELOPMENT PERMIT VERSION

PLAN LEGEND

PROJECT SITE BOUNDARY
 EXISTING WETLAND
 APPROXIMATED WETLAND BOUNDARY (NOT SURVEYED)
 POST CONSTRUCTION WETLAND BUFFER
 15' BSBL
 STREAM ORDINARY HIGH WATER MARK (OHWM)
 DIRECTION OF FLOW
 CLEARING & GRADING LIMITS/SILT FENCE

NOT FOR CONSTRUCTION

THESE PLANS HAVE BEEN
SUBMITTED TO THE APPROPRIATE
AGENCIES FOR REVIEW AND
APPROVAL. UNTIL APPROVED,
THESE PLANS ARE:
SUBJECT TO REVISION



Know what's **below**.
Call before you dig.

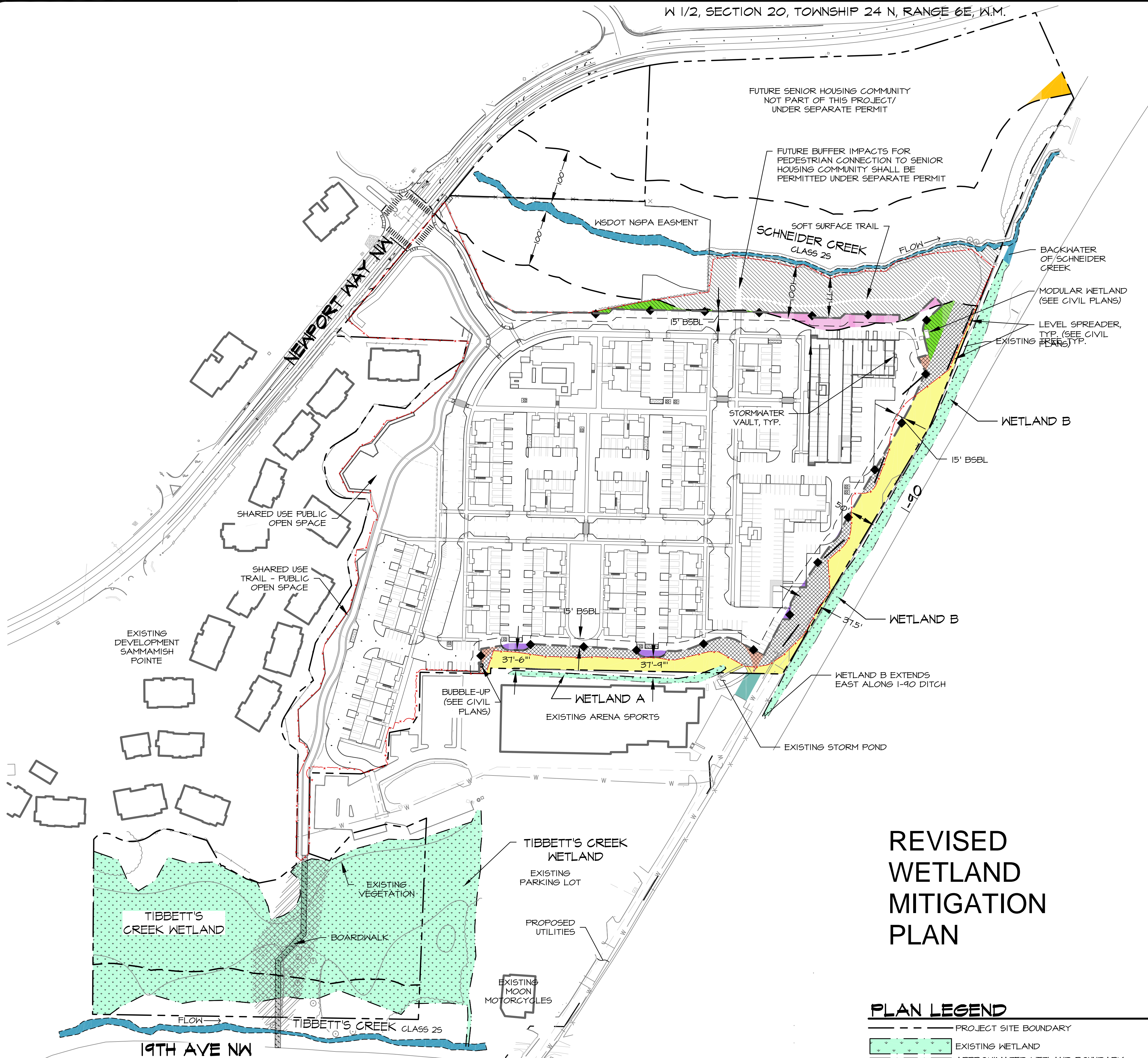
NOTES

1. SURVEY PROVIDED BY TRIAD ASSOCIATES, 20300 WOODINVILLE SNOHOMISH ROAD NE, STE. A WOODINVILLE, WA 98072, (425) 821-8448.
2. SITE PLAN PROVIDED BY VIA ARCHITECTS, 1809 7TH AVENUE STE. 800 SEATTLE, 98101, (206) 284-5624.
3. SOURCE DRAWINGS HAVE BEEN MODIFIED BY TALASAEA CONSULTANTS FOR VISUAL ENHANCEMENT.
4. THESE PLANS ARE ATTACHED TO THE CRITICAL AREAS STUDY & DETAILED CONCEPTUAL MITIGATION PLAN DATED JULY 2015.

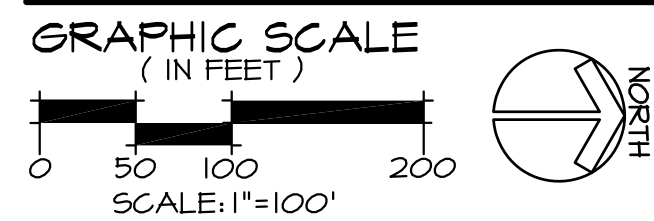
DETAILED CONCEPTUAL MITIGATION PLAN
PROPOSED SITE PLAN, IMPACTS & MITIGATION OVERVIEW PLAN
SSAQUAH GATEWAY
SSAQUAH, WASHINGTON

CITY COMMENTS	7-14-2015 AS
CITY COMMENTS	7-28-2015 AS
ate	4-28-20
scale	A5 SHOWN
signed	AO
rawn	ABS
checked	AO
approved	BS
project #	634C
meet #	W.I.

W 1/2, SECTION 20, TOWNSHIP 24 N, RANGE 6E, W.M.



PROPOSED SITE PLAN & IMPACTS & MITIGATION OVERVIEW PLAN



PLAN LEGEND

- PROJECT SITE BOUNDARY
- EXISTING WETLAND
- APPROXIMATED WETLAND BOUNDARY (NOT SURVEYED)
- POST CONSTRUCTION WETLAND BUFFER
- 15' B5BL
- STREAM ORDINARY HIGH WATER MARK (OHWM)
- FLOW → DIRECTION OF FLOW
- CLEARING & GRADING LIMITS/SILT FENCE

BUFFER AVERAGING LEGEND

WETLAND A & B BUFFER

REDUCED WETLAND BUFFER AREA	
WETLAND A BUFFER	1,133 SF
WETLAND B BUFFER	368 SF
TOTAL:	1,501 SF

REPLACED WETLAND BUFFER AREA	
WETLAND A BUFFER	1,147 SF
WETLAND B BUFFER	372 SF
TOTAL:	1,519 SF
(NET GAIN) 18 SF	

SCHNEIDER CREEK BUFFER

REDUCED SCHNEIDER CREEK BUFFER	4,650 SF
REDUCED BUFFER AREA FOR SOFT-SURFACE TRAIL	1,772 SF
TOTAL:	6,422

REPLACED SCHNEIDER CREEK BUFFER AREA	4,659 SF
REPLACED BUFFER AREA FOR SOFT-SURFACE TRAIL	1,772 SF
TOTAL:	6,431 SF
(NET GAIN) 9 SF	

BUFFER IMPACTS LEGEND

WETLAND BUFFER TEMPORARY CONSTRUCTION RELATED IMPACTS (GRADING/UTILITIES)	26,958 SF
OFF-SITE TEMPORARY CONSTRUCTION RELATED IMPACTS (UTILITIES)	1,251 SF
TOTAL:	28,209 SF

BUFFER MITIGATION LEGEND

STREAM BUFFER ENHANCEMENT	52,874 SF
WETLAND BUFFER RESTORATION FROM TEMPORARY CONSTRUCTION RELATED IMPACTS (GRADING/UTILITIES)	26,958 SF
RESTORATION FOR OFF-SITE TEMPORARY CONSTRUCTION RELATED IMPACTS (UTILITIES)	1,251 SF
WETLAND BUFFER ENHANCEMENT	30,563 SF
TOTAL:	111,646 SF

- NGPA SIGNS - SEE DETAIL 4, SHEET W2.1
- SPLIT RAIL FENCE - SEE DETAIL 3, SHEET W2.1

SHARED-USE PUBLIC TRAIL MITIGATION

BOARDWALK - INDIRECT CRITICAL AREA IMPACTS	
INDIRECT WETLAND IMPACTS	4,000 SF
INDIRECT WETLAND BUFFER IMPACTS	1,000 SF
WETLAND ENHANCEMENT (FOR INDIRECT IMPACTS FROM BOARDWALK) 4:1 (4,000 X 4 = 16,000 SF)	16,000 SF
WETLAND BUFFER ENHANCEMENT (FOR INDIRECT IMPACTS FROM BOARDWALK) 4:1 (1,000 SF X 4 = 4,000 SF)	4,000 SF

NOTES

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- SITE PLAN PROVIDED BY VIA ARCHITECTS, 1809 7TH AVENUE STE. 800 SEATTLE, 98101, (206) 284-5624.
- SOURCE DRAWINGS HAVE BEEN MODIFIED BY TALASAEA CONSULTANTS FOR VISUAL ENHANCEMENT.
- THESE PLANS ARE ATTACHED TO THE FINAL CRITICAL AREAS STUDY & FINAL MITIGATION PLAN DATED JANUARY 2016.



Know what's below.
Call before you dig.

APPROVED FOR CONSTRUCTION

BY:
CITY OF ISSAQUAH DEPARTMENT OF PLANNING

DATE:

FINAL CRITICAL AREAS MITIGATION PLAN
PROPOSED SITE PLAN, IMPACTS & MITIGATION OVERVIEW PLAN
GATEWAY APARTMENTS
ISSAQUAH, WASHINGTON

TALASAEA
CONSULTANTS, INC.
Resource & Environmental Planning
15000 Bear Creek Road Northeast - Woodinville, Washington 98077
Ph: (425) 861-7200 - Fax: (425) 861-7248

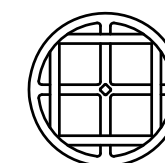
Revisions	By	Date
CITY COMMENTS	7-14-2015	ABS
CONSTRUCTION DOC	2-4-2016	ABS
4-28-2015		
AS SHOWN		
Drawn	ABS	
Checked	AO	
Approved	BS	
Project #	634C	
Sheet #	W1.1	



SUMMARY OF WETLAND BUFFER IMPACTS

GATEWAY APARTMENTS TAL634C

REVISED DETAIL PLAN OF
BUILDING 18 WETLAND B AREA



TALASAEA
CONSULTANTS, INC.

Resource & Environmental Planning

I-90 Green Edge Views with New Parking Lot SDP Approved Version

DESIGN CONCEPT: GATEWAY - EASTBOUND



- Added trellis elements in natural materials for distinction and Northwest aesthetic
- Increased depth of eaves incorporating traditional materials and heavy timber details
- Interesting and modulated roof form
- Traditional detailing of windows
- Incorporated and illustrated horizontal and vertical modulation

VIA

I-90 Green Edge Views with New Parking Lot SDP Approved Version

DESIGN CONCEPT: GATEWAY - WESTBOUND



- Evergreen landscape buffer from I-90
- Concealed parking
- Tall buildings at lowest part of site to the North – provide buffer without shading

I-90 Green Edge Views with New Parking Lot SDP Approved Version

VIEWS

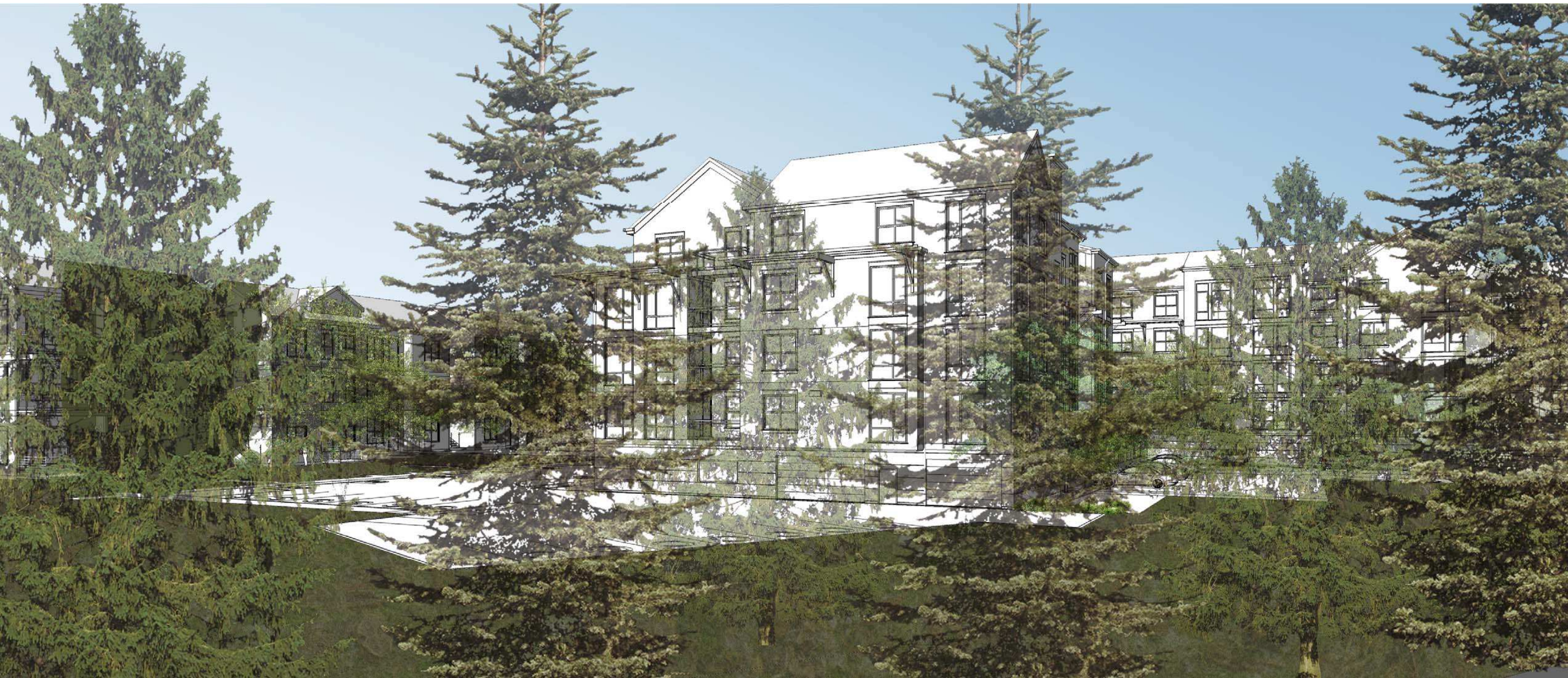


VIA

I-90 Green Edge Views with New Parking Lot - Proposed Revision



I-90 Green Edge Views with New Parking Lot - Proposed Revision



I-90 Green Edge Views with New Parking Lot Proposed Revision



ATTACHMENT 4 SDP15-00002 GATEWAY APARTMENTS

Public Comments to Proposed Revision to Parking along I-90

From: Connie Marsh
Sent: Wednesday, June 08, 2016 9:06 AM
To: Lucy Sloman; Amy Tarce
Cc: Mary Lynch; Keith Niven; Peter Rosen
Subject: Where is the SEPA addendum in the proposed move to surface parking for Gateway Apartments?

Good Evening Development Commission,

I am perplexed that more surface parking would be allowed after the plan was approved. The CIP has a large base in reducing surface parking, often citing the expansive surface lots as a huge negative, yet after a plan is agreed upon the City is looking at allowing more surface parking?

It is unclear why this is needed, and it is unclear how this is an improvement given the overall context of the CIP.

Yes, it all may be within the standards of the Central Issaquah Plan but SEPA still controls, and impacts to critical area buffer requires that the impacts first be shown that they cannot be avoided. Where is this conversation? Next the impacts need to be shown to be minimized. Where is this conversation? Finally, if there is no other alternative, which there clearly is, the impacts can be mitigated.

Please do not agree to this change to surface parking. See further language below from the CIP and the CIDDS.

Thanks,

Connie Marsh

Central Issaquah Plan

Urban Community Recommended Implementation Strategies

2. discouraging off-street surface parking and limiting their location to rear or side yards along the street frontage only when under building parking is not accessible; and....

UC Policy C4. Consider initiating a Parking District strategy by: a. creating additional Parking Districts as redevelopment occurs throughout the Plan area to provide adequate vehicular and bicycle parking for uses within each District; and, b. providing incentives to encourage a transition from surface parking to structured parking.

UC Policy A5. Integrate, landscaping, courtyards, plazas, public art, and critical areas and buffers into developments to enrich the urban landscape and establish a sense of place.

8.1 Parking Standards

Intent

The intent of this Chapter is to establish standards for the design, configuration and performance of parking facilities based on urban densities and needs. Parking encompasses all public and private facilities necessary for the storage of motorized and non-motorized transportation and encourages the use of parking garages rather than surface parking, and supports a pedestrian-friendly environment and attractive urban design. The purpose of requiring parking as a condition of development is to provide an adequate amount of parking for a site, recognizing that a balance must be reached between: Insufficient Parking, leading to overflow parking in adjacent streets and neighborhoods, abutting streets as well as unauthorized parking in nearby private lots or bicycles chained to streetlights and benches; and, Excessive Parking, wasting space and resources that could be better utilized for people, landscape, etc. These Development standards support the Design standards found in Chapter 15 Parking, and are intended to:

A. Ensure adequate, safe, and reasonable storage of and access to parking/facilities B. Allow for flexibility in the design and location of parking/facilities; C. Efficiently and effectively use the site and the parking provided therein; D. Encourage the use of on-street parking and allow it to meet as much of the required parking as possible; Development Standards: 8.0 Parking Standards Updated Ord 2754, 12-22-15 Page 2 of 23 E. Encourage the use of other urban, more pedestrian-friendly forms of parking (such as on-street parallel parking, structured parking, etc...), rather than on-site surface parking lots, to meet as much of the required parking as possible; F. Provide facilities appropriate for the anticipated use with a minimum of paving; and, G. Allow flexibility to adapt to changing market needs, including car sharing, and different modes of transportation, and implement changing community priorities.

From: M Lynch <melynchwa@yahoo.com>
Sent: Wednesday, June 08, 2016 1:21 PM
To: Lucy Sloman; Amy Tarce
Cc: Keith Niven; Peter Rosen; Connie Marsh
Subject: Re: Where is the SEPA addendum in the proposed move to surface parking for Gateway Apartments?

Lucy

1. So it does not have to be posted on the Active project list either?
2. What is the impact on storm water run off quantity with this added parking surface and reduction in buffers?
3. Will you send me a copy of the revised storm water runoff study for this and the added water for the artisan spring called out in the SEPA Addem.? There currently is still no storm water study posted on the active project list.
4. Also when can one expect to see an updated traffic study? With the SEPA Addem and added construction traffic would like to see a copy of the that to see how decisions were made and what is to come with Newport Way.

Thank you.

Mary Lynch

From: Lucy Sloman <LucyS@issaquahwa.gov>
To: Amy Tarce <AmyT@issaquahwa.gov>; 'M Lynch' <melynchwa@yahoo.com>
Cc: Keith Niven <KeithN@issaquahwa.gov>; Peter Rosen <PeterR@issaquahwa.gov>; Connie Marsh <auntgrumpy@comcast.net>
Sent: Wednesday, June 8, 2016 12:11 PM
Subject: RE: Where is the SEPA addendum in the proposed move to surface parking for Gateway Apartments?

Mary

To clarify, this was determined to be a minor change or Level 0 and a Level 0 permit does not require public notice.

Lucy



Lucy Sloman AICP

Land Development Manager & Designated Official for the Urban Villages
Development Services Department
City of Issaquah
PO Box 1307 (mail)
1775 12th Ave NW
Issaquah, WA 98027
425|837-3433 direct
425|837-3080 fax
lucys@issaquahwa.gov new as of Nov 2012

From: Amy Tarce
Sent: Wednesday, June 08, 2016 12:05 PM
To: 'M Lynch'
Cc: Keith Niven; Peter Rosen; Connie Marsh; Lucy Sloman
Subject: RE: Where is the SEPA addendum in the proposed move to surface parking for Gateway Apartments?

Mary,
As the Staff Memo to the Development Commission stated, this minor revision is a Level 0 review and we made the determination that this did not warrant public notice.

Amy Tarce, AICP, Assoc. AIA
Senior Planner
City of Issaquah
425.837.3097 direct

From: M Lynch [<mailto:melynychwa@yahoo.com>]
Sent: Wednesday, June 08, 2016 10:24 AM
To: Connie Marsh; Lucy Sloman; Amy Tarce
Cc: Keith Niven; Peter Rosen
Subject: Re: Where is the SEPA addendum in the proposed move to surface parking for Gateway Apartments?

Also would ask why the parties of records have not gotten a copy of the notice as of today 8June2016.

Mary Lynch

From: Connie <auntgrumpy@comcast.net>
To: Lucy Sloman <LucyS@issaquahwa.gov>; Amy Tarce <amyt@issaquahwa.gov>
Cc: Mary Lynch <melynychwa@yahoo.com>; Keith Niven <keithn@issaquahwa.gov>; Peter Rosen <PeterR@issaquahwa.gov>
Sent: Wednesday, June 8, 2016 9:05 AM
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Connie Marsh

Central Issaquah Plan

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- D. Encourage the use of on-street parking and allow it to meet as much of the required parking as possible; Development Standards: 8.0 Parking Standards Updated Ord 2754, 12-22-15 Page 2 of 23
- E. Encourage the use of other urban, more pedestrian-friendly forms of parking (such as on-street parallel parking, structured parking, etc...), rather than on-site surface parking lots, to meet as much of the required parking as possible;
- F. Provide facilities appropriate for the anticipated use with a minimum of paving; and,
- G. Allow flexibility to adapt to changing market needs, including car sharing, and different modes of transportation, and implement changing community priorities.